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PROPOSED GENERAL DEVELOPMENT PLAN

FOR THE

**KYRENE INDUSTRIAL DISTRICT**

PREPARED BY THE MARICOPA COUNTY PLANING AND ZONING DEPARTMENT

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PROPOSED GENERAL DEVELOPMENT PLAN

for the

KYRENE INDUSTRIAL DISTRICT

Prepared for the

GILA RIVER INDIAN RESERVATION

By

THE MARICOPA COUNTY PLANNING & ZONING DEPARTMENT

October 1961

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103 WEST JEFFERSON  
PHONE AL 8-9611  
PHOENIX, ARIZONA

October 13, 1961

Mr. Minton J. Nolan  
Superintendent  
Bureau of Indian Affairs  
Pima Agency  
Phoenix, Arizona

Dear Mr. Nolan:

I am pleased to submit herewith a Proposed General Development Plan for the Kyrene Industrial District. This report is in accordance with a planning program agreement dated June 29, 1961, and your letter of approval dated July 7, 1961. This report was prepared pursuant to authorization by the Planning and Zoning Commission and the Board of Supervisors of Maricopa County.

The report includes a comprehensive analysis of the existing conditions and a generalized land-use plan for the physical development of certain lands within the Gila River Indian Reservation. The plan also contains a system of major arterials, minor access roads to serve the industrial park, and our recommendation for an important interchange to be located on the proposed Interstate Route 10.

We hope that this report is sufficient to now enable you to move ahead into the next stage of implementation as outlined in the Gutenberg Report. We have enjoyed working on this interesting assignment and are grateful for the assistance and cooperation received from your office, the Arizona State Highway Department, and the Maricopa County Highway Department.

Very truly yours,

A handwritten signature in cursive script that reads "Donald W. Hutton".

Donald W. Hutton, Director

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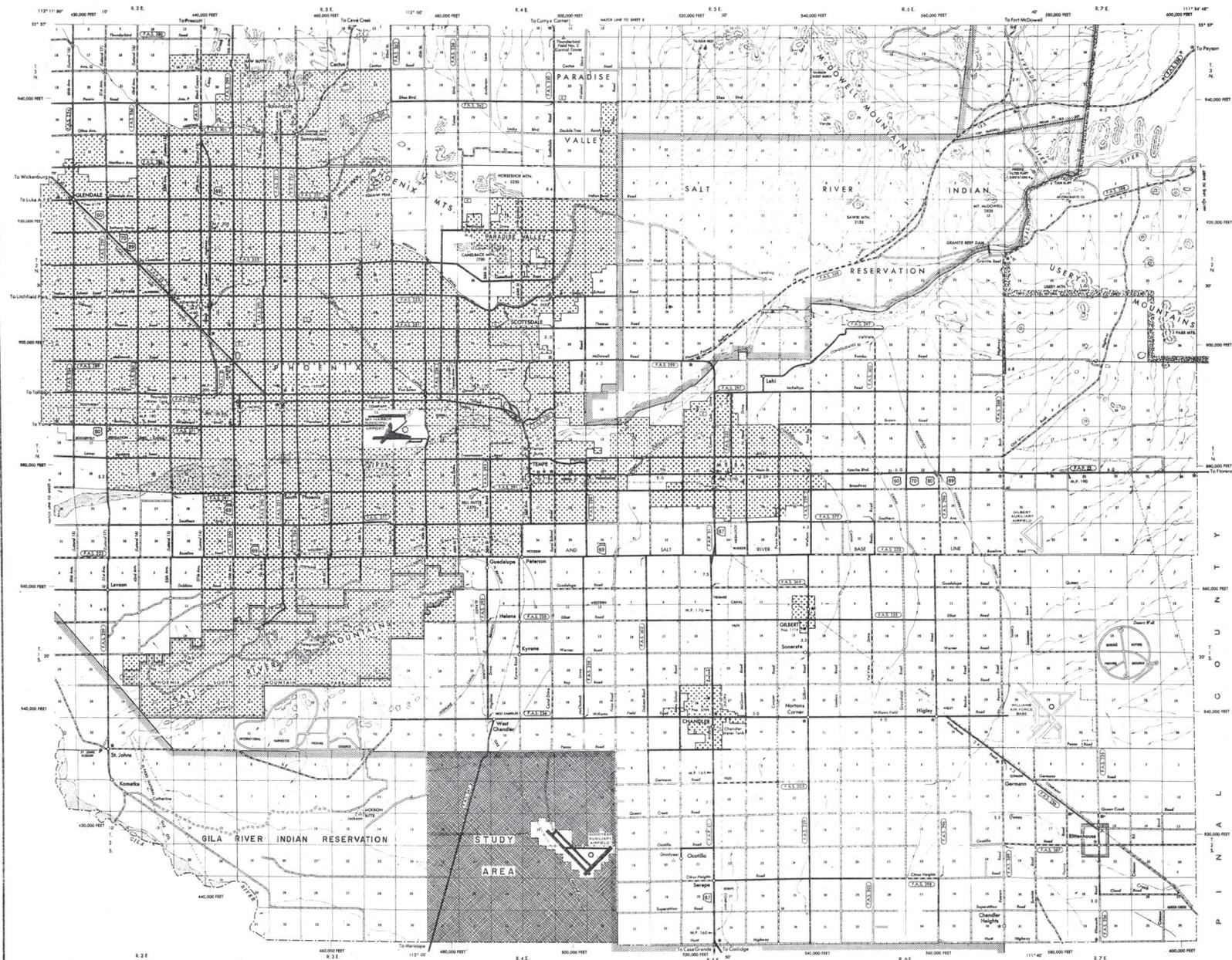
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## INTRODUCTION

### Background

In August 1960, the U.S. Bureau of Indian Affairs received an economic analysis report prepared by Arthur Gutenberg & Associates concerning the proposed Kyrene Industrial Park to be located on a portion of the Gila Indian Reservation. The report points out the economic potential existing in the Salt River Valley, specifically, the industrial possibilities of a thirty-six square mile area (T2S, R4E, Gila and Salt River Base and Meridian) located near the northern boundary of the reservation. This report is the result of one of the recommendations made in the report:

"Prior to the leasing of land, a master plan should be developed which will allocate the land according to the needs of obnoxious industries, testing facilities, community services, etc."

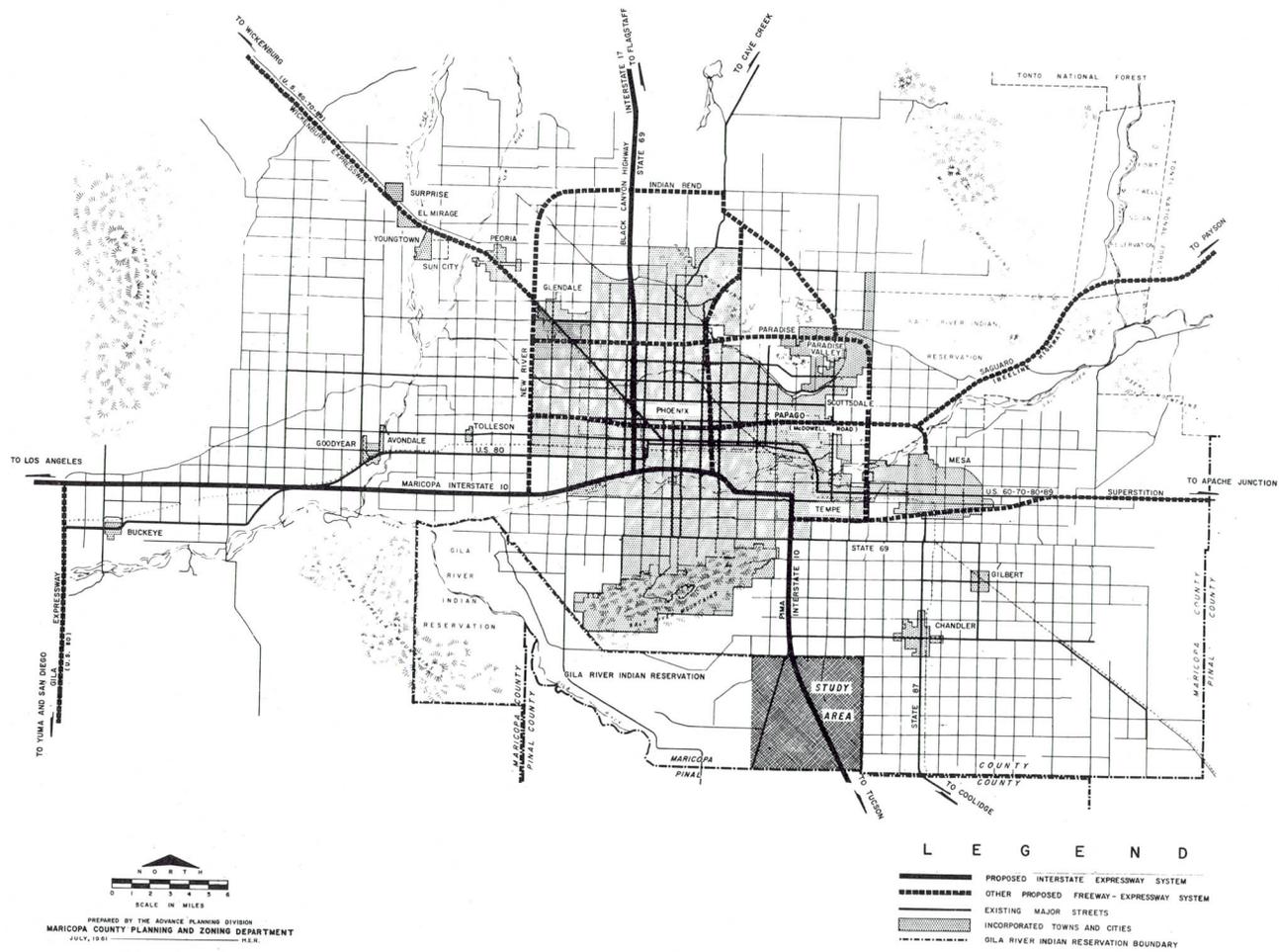


TRANSVERSE MERRIAM PROJECTION ARIZONA CENTRAL ZONE  
 COMPILED BY PHOTOGRAMMETRIC METHOD  
 REF. NORTH AMERICAN DATUM  
 CONTROL BY U.S. COAST AND GEODETIC SURVEY, U.S. GEOLOGICAL SURVEY  
 U.S. FOREST SERVICE AND U.S. GENERAL LAND OFFICE  
 FEBRUARY 1954  
 COPYRIGHT ARIZONA HIGHWAY DEPARTMENT 1955

# GENERAL HIGHWAY MAP

PLATE NO. 2





# VICINITY MAP

PLATE NO. 1

## CHAPTER 1 EXISTING CONDITIONS

### Location

The proposed Kyrene Industrial Park is located in Township 2 South, Range 4 East. It is a flat non-habitable area in the extreme northeast corner of the Gila Indian Reservation and lies approximately twenty miles southeast of downtown Phoenix. Plate 1 shows the location of the industrial park and its relationship to the adopted major street and highway plan for the Phoenix Urban Area-Maricopa County.

The proposed Pima Expressway (Interstate Route 10) passes directly through the proposed Kyrene Industrial Park, and is one of the major assets in the area. The interstate route will provide fast and easy access to downtown Phoenix, and direct connections to Tucson, San Diego, Los Angeles, and eventually to all other major U.S. cities. Plate 2 shows the location of the proposed Kyrene Industrial Park in relationship to the existing State and County Highway System.

### Close By Industrial Areas

There is only one general area close to the proposed Kyrene Industrial Park which now contains any significant amount of industrial development; it is situated south of Baseline Road between 56th Street and Kyrene Road.

This area is commonly referred to as The Kyrene Industrial Area due to the existence of several heavy industrial uses now operating in the area. They include: National Casting Company, Yuba Steel Company (fabrication), Armco Metal Products Company, and the Salt River Steam Generating Plant. Other close by industrial developments include the S.W. Nitrate Chemical Plant and the Serape Cotton Oil Company located south of Chandler on Highway 87. (See land-use map)

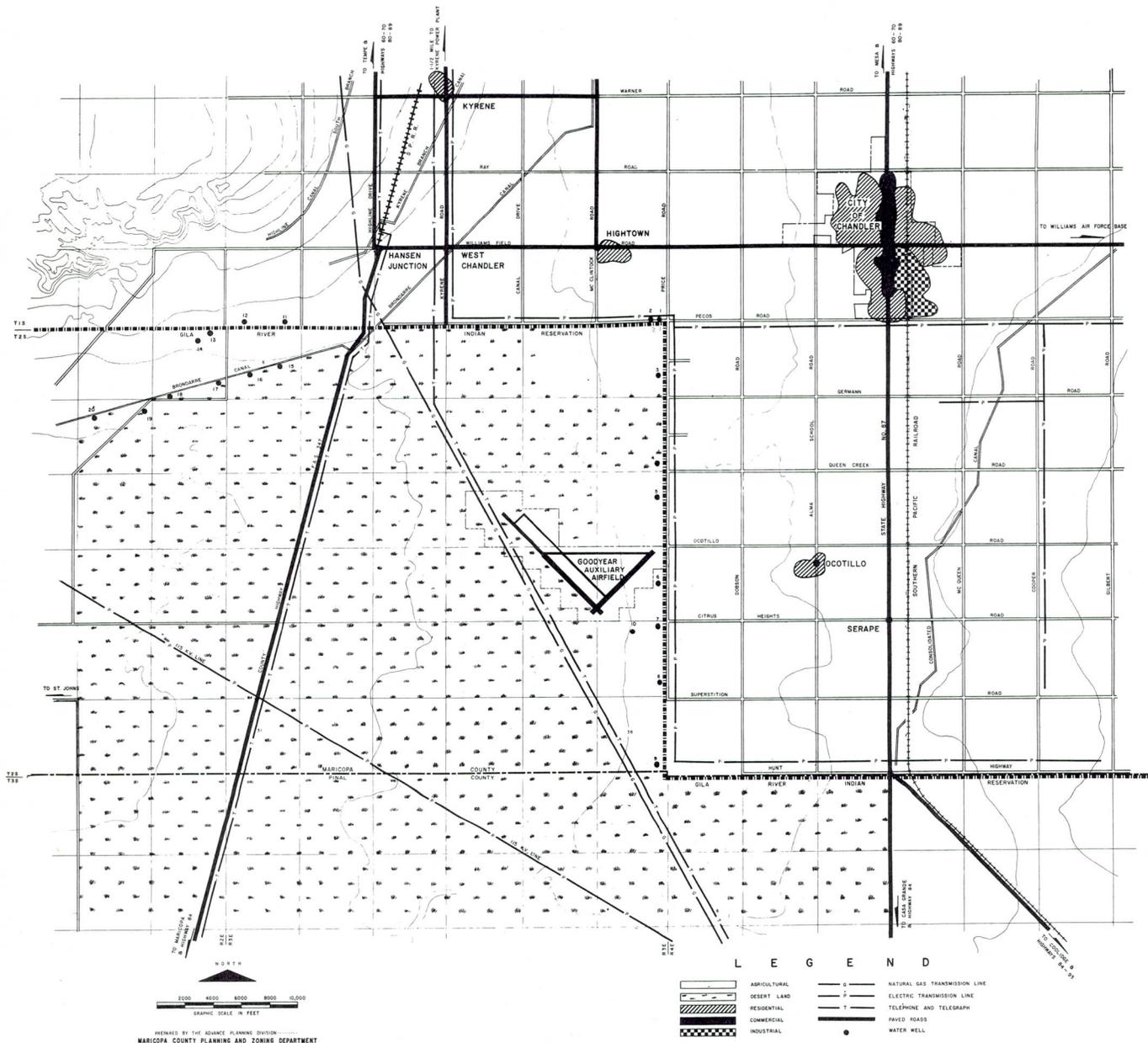
The need for land for industrial development within the Phoenix Urban Area is expected to be more than doubled by 1980. There are 4,763 acres of land currently in use for industrial purposes, and an expected 1980 demand of 11,555 acres based on a projected 1980 population of 1,400,000 for Maricopa County.

The following table shows the amount of land anticipated for industrial purposes by 1980 for specific towns and cities in Maricopa County. These figures are taken from planning reports recently prepared by the Advance Planning Division of The Maricopa County Planning Department.

TABLE 1  
ESTIMATED FUTURE INDUSTRIAL AREA REQUIREMENTS\*

City	1980 Industrial Land Requirements	Projected 1980 Population
Phoenix	8,800 Acres	1,000,000
Scottsdale	300	86,000
Tempe	1,950	
Mesa	754	130,000
Chandler	148	20,200
Gilbert	112	<del>35,000</del> 3,500
Total	12,064 Acres	

\* Based upon existing population and land-use ratios.



# EXISTING CONDITIONS GILA RIVER INDIAN RESERVATION

### Ownership

The township under study consists mostly of allotted lands of ten-acre tracts and those classified as tribal lands. Approximately 1,200 acres are leased by the U.S. Government and used by the Williams Air Force Training Base as an auxiliary airstrip. Other portions of the area are leased by private individuals for agricultural purposes.

It is not within the scope of this report to analyze and make recommendations upon problems that relate to land ownership here.

### Land Use

The existing conditions are shown on Plate 3 of this report. Most of the township is relatively flat and covered with desert brush, with the exceptions of Sections 1, 2, 13, 25, 26 and part of 24 which is irrigated and in agricultural use.

The study area slopes gently to the west falling approximately ten feet to the mile. A large number of small washes ranging from two to six feet in depth are scattered throughout the area.

### Soils

Present soil conditions in the area are classified as Number 7 with a potential of from 2 to 4 by the Soil Conservation Department. The land appears to be suitable for growing cotton, grasses, and sorghums if the proper irrigation and farming methods were employed.

A complete soil analysis should be made as to soil-bearing capacity before construction of roadways, rail spurs or buildings.

#### Surface Drainage

No flood or watershed studies exist pertaining to the study area nor are there any contemplated by the County Flood Control, Soil Conservation Service or the Army Corps of Engineers at the time of this writing.

Some occasional flooding occurs south of the airfield but is not considered to be a serious obstacle to the plan, if the Soil Conservation Service follows through with their proposals to construct a series of diversion dams east of the reservation.

TABLE 2  
WATER CHARACTERISTICS

Well No.	Total Solvable Salts (Parts Per Million)
1	5,143
2	4,312
3	2,358
4	3,601
5	2,433
6	3,510
7	1,878
8	3,159
9	1,903
10	1,104
11	2,617
12	2,942
13	1,536
14	2,550
15	3,658
16	4,147
17	3,827
18	4,064
19	4,124
20	3,722

Source: Department of Interior, Soil Conservation Branch, Sacaton, Arizona.

## CHAPTER 2

### UTILITIES

#### Water

All water will be supplied from underground resources, since the area is presently excluded from existing irrigation districts. This source is available and appears to be adequate. However, from samples and data collected from existing wells in the area by the Soils Department of Indian Affairs, Sacaton, a high salt content is present throughout the general area. Plate 3 shows the location of existing wells near the study area. Table 2 shows the average total solvable salt content of each well from samples taken annually during the past four years or since the well has been put into operation. It also shows that the total solvable salts vary from 1,100 to as high as 5,100 parts per million. The State Board of Health accepts 500 to 1,000 as being suitable for drinking purposes and has recently turned down applications for domestic use in the Kyrene Area.

The City of Buckeye has an experimental water treatment plan operating in their community. It is owned and operated by Ionics Inc. of South Pasadena, California, and has been reducing the salt content from 1,000 to 300-400 parts per million at a cost of fifty to sixty cents per thousand gallons. This is high compared to the City of Phoenix, which has a rate of approximately thirty cents or less per thousand gallons.

A complete breakdown of type and per cent of salts for each well can be obtained from the water branch of the Soil Conservation Department, Department of Interior, Sacaton. It is recommended that a complete engineering analysis of the water, methods and costs of treatment for industrial and domestic consumption be completed before any development of the area is contemplated.

#### Sewage

Sewage disposal facilities for the area need not be a problem if County health requirements are met. New industries requiring low water consumption and disposing of small quantities of waste could provide their own individual sewage disposal systems. A community system rather than individual sewage devices should be provided in conjunction with any future large-scale residential development.

The location of any major treatment plant should be carefully located to avoid noxious odors from being carried by winds over residential or industrial areas.

#### Power

Electrical power is supplied to the area by the Salt River Power District but could be wholesaled by a separate power agency if desired by the Indian Reservation.

A high voltage grid system around the entire valley is now under study which will be connected to the Federal Bureau's 115 KW line of which part lies in the southwest corner of the study area. A portion of this grid system could be located advantageously to any development selected if the Salt River Power District officials are notified early enough.

### Gas Service

Gas service to the area comes under the jurisdiction of the Arizona Public Service Company. At present, they have lines at Ocotillo and west of Chandler which could be easily extended to the area.

The El Paso Gas Company has three large high pressure natural gas transmission lines running through the area and would be available for service connection should a large demand occur in the near vicinity. The location of these high pressure lines has been a factor in the design of the street system and lot pattern in the area.

### Communications

The telephone and telegraph service is provided by the Mountain States Telephone and Telegraph Company. At present, they have a line running parallel to the existing County Road and foresee no problem of extending their services into the area. There also exists an AT&T coaxial cable running through the study area as shown on Plate 3 located in an easement which lies parallel with the El Paso Gas Line easement.

### Highways

At the present time, access through the area is provided by a single County highway (F.A.S. 347) and several unimproved roads maintained by the Indian Reservation. Until new development occurs, the present system appears to be adequate.

### Railroads

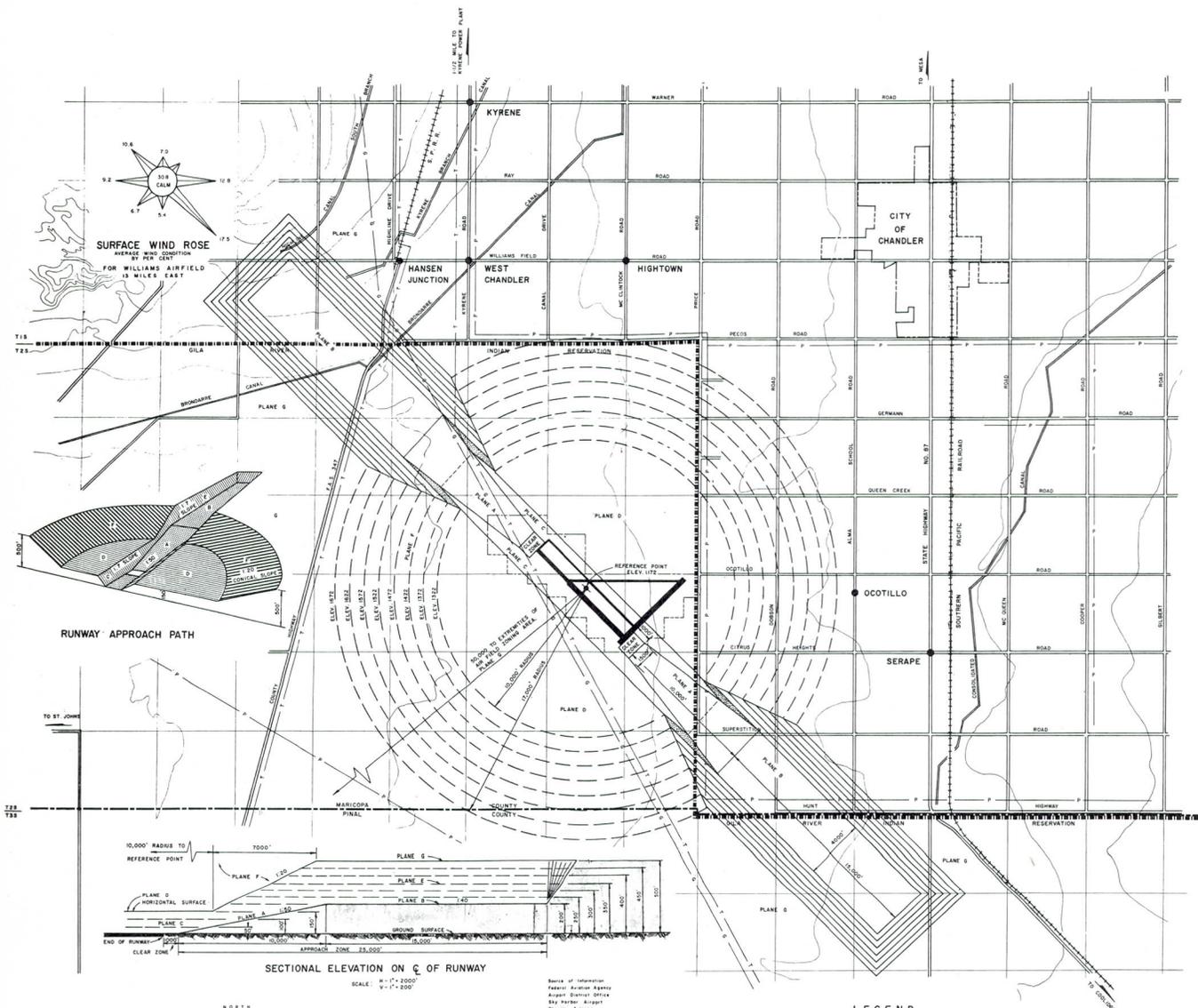
The Southern Pacific Railroad has a spur track located immediately north of the study area terminating just south of Williams Field Road and Highline Drive. This spur connects with the mainline eight miles north in Tempe; it provides direct access to Los Angeles, San Diego, Tucson, and El Paso. The Southern Pacific line connects directly to the Santa Fe lines at Phoenix giving direct service to other midwest and eastern railroads.

The Southern Pacific Railroad Company offers free service in preparing engineering studies and cost estimates to developers using the Southern Pacific lines and their extensions for industrial uses.

### Air Facilities

One of the most important assets to the area is the existing Goodyear Auxiliary Airfield facility which could easily be converted and used for both private and public use. The northwest-southeast runway has been well maintained and is in excellent condition. It is slightly under 10,000 feet in length and can handle most types of aircraft. The shorter east-west runway, taxiways and aprons have been neglected and will require some repair before they can be used. The runways and aprons have a good sub base capable of withstanding a load capacity of approximately 25,000 pounds per square foot.

The Goodyear Field is presently being used by the Williams Air Force Base as part of their training program and expected to be used as a training facility as long as Williams Air Force Base is needed. However, some joint use of the airfield might be possible if properly regulated and private use held to a minimum.



PREPARED BY THE PLANNING DIVISION  
 MARICOPA COUNTY PLANNING AND ZONING DEPARTMENT  
 JULY, 1961

# EXISTING AIR FIELD ZONING PLAN

PLATE NO. 5



This facility is within fifteen miles from Sky Harbor Airport and considered to be one of the four major air facilities in the valley. Plate 4 shows the relationship of the Goodyear Auxiliary Field to the other airfields within the Phoenix Metropolitan Area.

Plate 5 shows the existing airfield zoning plan adopted by the Maricopa Board of Supervisors in 1956. The plan is also approved and adopted by the Federal Aviation Agency. Its major purpose is to limit the construction and the height of buildings in various areas that would create hazardous conditions to aircraft making an approach to the field.

## CHAPTER 3

### PLANNED INDUSTRIAL DISTRICTS

There were approximately 1,000 so-called planned industrial districts in the United States in 1960, with probably fewer than 600 being bona fide to be called planned industrial districts according to a recent report published by the Urban Land Institute. The following definition incorporates elements of earlier definitions published by the Urban Land Institute and the Department of Commerce and considered to be applicable to the proposed Kyrene Industrial District:

" An organized or planned industrial district is a tract of land which is subdivided and developed according to a comprehensive plan for the use of a community of industries. The plan includes detailed provision for streets, railroad tracks and utilities, either installed before the sites are sold or otherwise assured to prospective occupants.

The comprehensive plan must insure adequate control of the area and buildings through zoning, private restrictions incorporated as legal requirements in deeds of sale or leases, and the provision of continuing management, all with a view to protecting the investments of both developers of the district and industries occupying the improved sites. The management handles negotiations with local governmental authorities on behalf of the tenants, and it may erect buildings prior to sale or lease for speculative purposes." \*

---

\* "A Report on the Dartmouth Conference on Industrial Parks, prepared by William Lee Baldwin, sponsored by Arthur D. Little, Inc. and the State of New Hampshire, June, 1958.

### Design and Development Standards

Design standards for planned industrial districts vary considerably. This is largely due to the fact that the planned industrial district concept has only recently become popular and sufficient time has not elapsed to draw conclusions for a basis for establishing uniform standards. Another reason is that the development objectives and characteristics vary with each district.

The Urban Land Institute has recently completed a comprehensive survey of industrial districts now in operation in the United States. This report was helpful in establishing the standards set forth herein but should only be considered preliminary and subject to modifications as warranted by changing conditions.

### Lot Size

Because of the various land-area requirements by each industry, lots should not be designated on the final plat at time of recording. However, plats should show all other pertinent information generally required of residential plats.

Table 3 is the result of a nation wide survey taken to determine the lot area requirements of industries located within a planned industrial district. Based upon this survey, it can be assured that the manufacturing establishments will require the larger sites ranging in size from one to five acres in size with an average requirement of around three acres. The non-manufacturing establishments will require the smaller sites ranging in size from one-half acre to three acres, with a one and one-half acre site being the most frequently used. The depth of the sites shown on Plates 7 and 8 range from 200 feet in depth. This is considered to be adequate to meet the requirements of most industries. Those industries requiring greater depth and size can be accommodated adjacent to the airport where there is sufficient space.

TABLE 3  
INDUSTRIAL LOT SIZES\*

Lot Size (Net Acres)	Number of Occupants		% of Total Area
	Manufacturing	Non-Manufacturing	
0 - 4.9	379	705	71.2%
5 - 9.9	117	90	13.6
10 - 24.9	97	54	9.9
25 - 49.9	32	14	3.0
50 - 99.9	9	7	1.1
100 - 99.9	13	4	1.1
1,000 -	1	1	0.1
Total	648	875	100.0%

\* "Industrial Districts Restudied, An Analysis of Characteristics," Urban Land Institute Technical Bulletin 41, April 1961.

#### Block Length and Width

The block length and street patterns should be designed to conserve land, permit the best possible arrangement of facilities and improvements, to assure maximum accessibility, and allow some degree of flexibility. The block lengths shown on Plate 6 vary between 1,000 and 2,400 feet in order to provide the maximum number of industrial sites and development costs to a minimum. Plates 7 and 8 represent enlargements in scale of "Industrial Areas A and C" shown on Plate 6. These plans show in better detail the suggested block, lot and street patterns. Plate 7 also shows the recommended street cross section recommended for use in the Kyrene District.

### Rail Facilities

The Engineering Department of the Southern Pacific Railroad Company would have the responsibility of serving the district with rail lead tracks and industry spurs. The right-of-way width requirements vary from 17 feet for a single-track lead to a minimum width of 53 feet for a double-track lead.

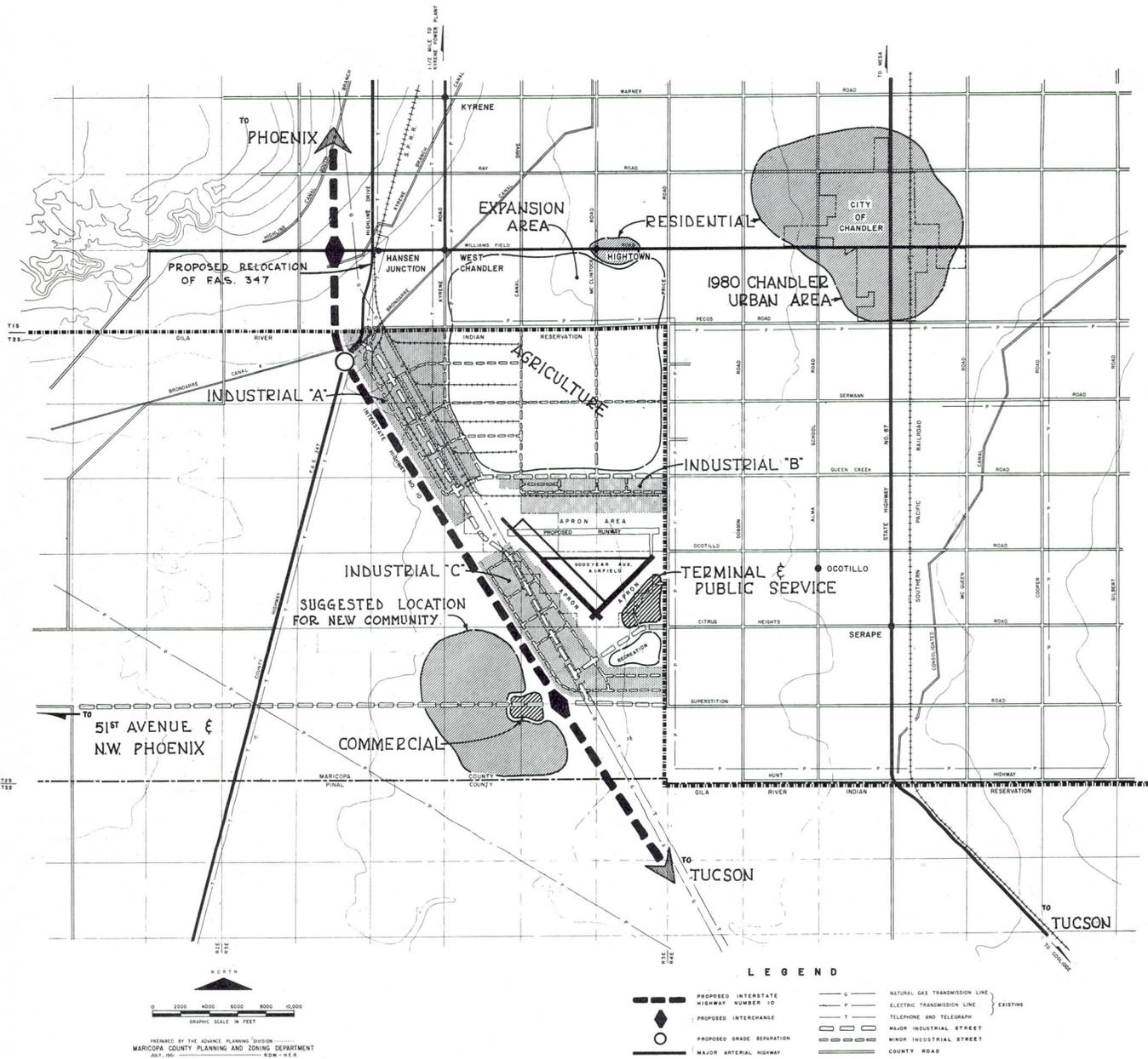
### Physical Improvements

The physical improvements of the tract includes grading, street surfacing, rail lead tracks, utilities and landscaping. The scheduling and specifications required for the initial development should be determined and should meet all Maricopa County Health and Highway standards. The exact location within the district should also be carefully considered and based on the amount of land estimated to be fully developed over a five-year period of time.

### Other District Development Standards

Existing zoning ordinances seldom offer the full protection to the developers and occupants of industrial districts. In order to fully meet the development objectives, private restrictive covenants are frequently used elsewhere in conjunction with planned industrial districts.

The restrictions contained in covenants usually cover such things as setback distances from roads, establishment of building lines, off-street parking and loading requirements, standards of construction, landscaping and types of industries permitted. It is recommended that restrictives of this type be adopted for the Kyrene Industrial District. A set of typical restrictive covenants is found in Appendix A of this report.



# PROPOSED GENERAL DEVELOPMENT PLAN

PLATE NO. 6

PREPARED BY THE ADVANCE PLANNING DIVISION  
 MARICOPA COUNTY PLANNING AND ZONING DEPARTMENT  
 JULY, 1961 R.W. KEK

CHAPTER 4  
GENERAL DEVELOPMENT PLAN

The General Development Plan for the Kyrene Industrial District is shown on Plate 6. It encompasses approximately thirty-six square miles of land, 2,500 acres of which has been planned for industrial development. The remaining acreage has been carefully analyzed and designated for future residential, recreational, commercial, agricultural or desert uses. The plan has taken into consideration the existing conditions including existing land use, highway system, topography, utilities, rail and air facilities, and location of the proposed Interstate Route No. 10. The latter two being the most significant.

Proposed Interstate Route No. 10

The proposed interstate route is situated just west of the Goodyear Auxiliary Airstrip, traversing in a southwest direction through the Indian Reservation ultimately providing a limited access highway facility built to expressway standards. This route is an important part of the National System of Defense Highways which will link together all the major cities in the United States and carry twenty per cent of all the generated vehicular traffic. A diamond interchange is proposed at the junction of Williams Field Road providing direct access to the City of Chandler and Williams Air Force Base. This facility will also provide good access to the northern portion of the Kyrene Industrial District. A second interchange is

proposed at the junction of Superstition Road providing excellent access to the southern portion of the district as well as making possible a desirable County by-pass route to 51st Avenue. These interchanges are reasonably spaced to provide the maximum benefits to the adjoining lands and Maricopa County. The general route alignment and interchange locations have been reviewed with officials of the State Highway Department.

#### Proposed Major Street Pattern

A preliminary major street and highway system designed to serve the district is shown on Plate 6. This system is designed to accommodate both internal and through vehicular traffic. Typical street cross sections recommended for use are shown on Plate 7. Through traffic will be accommodated on the major streets and access to the various industrial sites will be provided on the minor streets.

The street system was designed to utilize the small land space situated between the El Paso Gas Company and American Telephone and Telegraph Company utility easements. The close relationship of the airstrip to the interstate route was another basic factor controlling the street pattern.

#### Rail Lead Tracks

The close proximity of the Southern Pacific lead tracks is a valuable asset to the Kyrene District. These tracks may be easily extended into the district should they be desired and sufficient development warrants the expense. The Proposed General Development Plan shows how these facilities might be extended into the district. The plan proposes that the tracks be extended south from their most terminal point in a southeastern direction running parallel to and near the existing American Telephone and Telegraph and El Paso Gas Utility easements. Additional lead tracks

could branch off to the east or west when and if the future developments warrant.

It is further recommended that the existing jog occurring on Highline Drive (F.A.S. 347) be eliminated, thus, providing a more direct route and avoiding a double grade crossing at the junction of Williams Field Road.

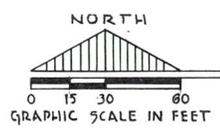
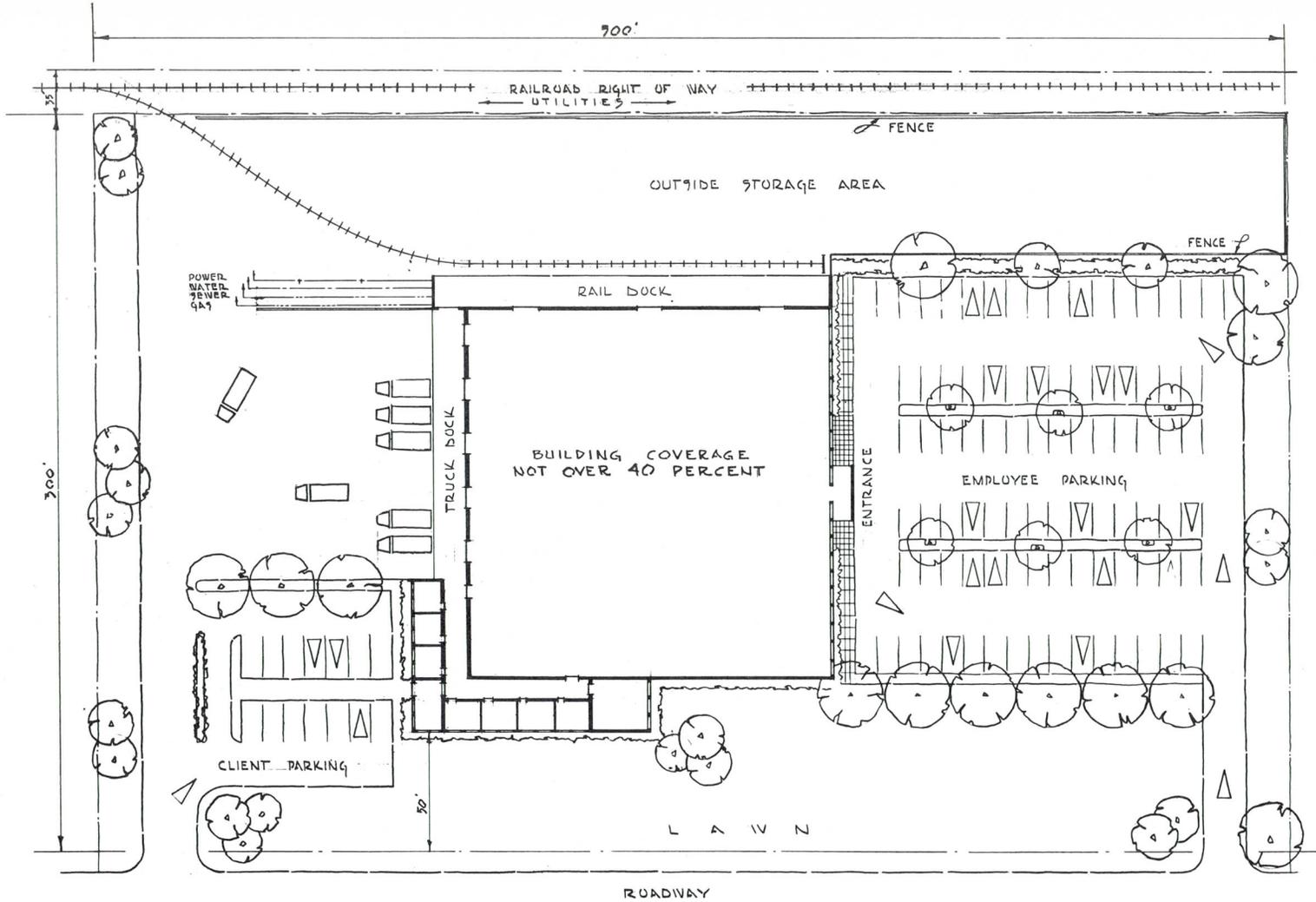
#### Goodyear Auxiliary Airfield

It is presumed that the Goodyear Auxiliary Airfield will eventually be used for military, public and private use. The need for more air facilities in the valley has already been recognized by the Federal Aviation Agency. The Goodyear Auxiliary Airfield is a valuable facility that should be used to help meet the need. The General Development Plan is based on the assumption that the facility will eventually be used by both public and private enterprise in addition to the military.

The plan provides for the possible installation of a new east-west airstrip and other needed terminal facilities including some form of open recreational facility. This facility would be of benefit to the entire industrial district.

#### Industrial Areas A, B and C

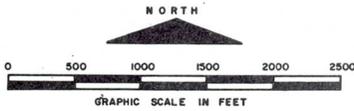
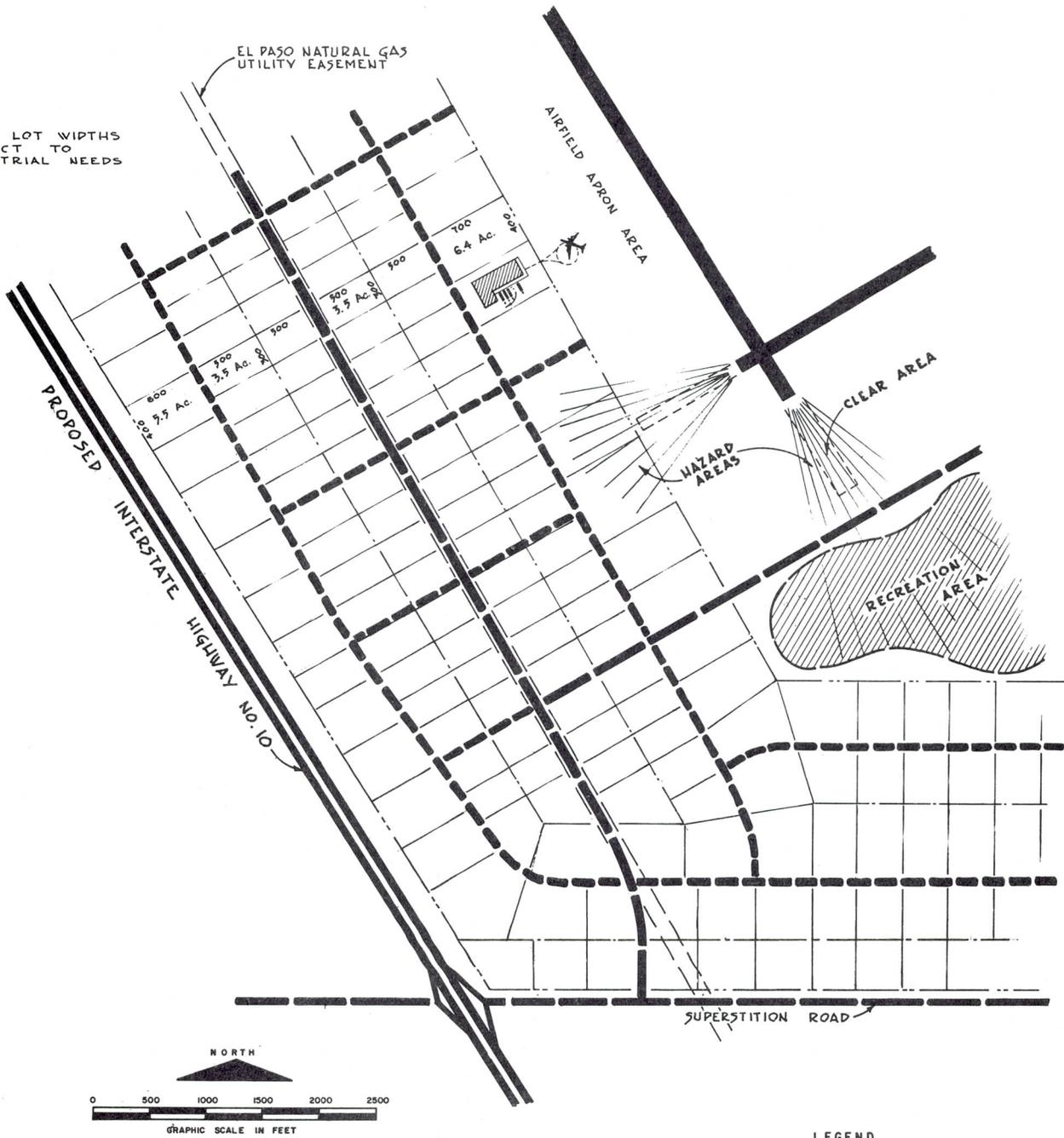
The industrial district plan identifies three specific areas; each have certain advantages and disadvantages over the other, depending on the needs of each industry. Industrial Area A would be more attractive to industries having a need for rail and highway access. Industrial Area B could best serve those industries with a need for close-by air facilities. Area C would best be suited for those industries relying only upon air and highway



# typical site plan

PREPARED BY MARICOPA COUNTY PLANNING DEPARTMENT  
AUGUST 1961 H.E.R.

**NOTE:**  
EXACT LOT WIDTHS  
SUBJECT TO  
INDUSTRIAL NEEDS

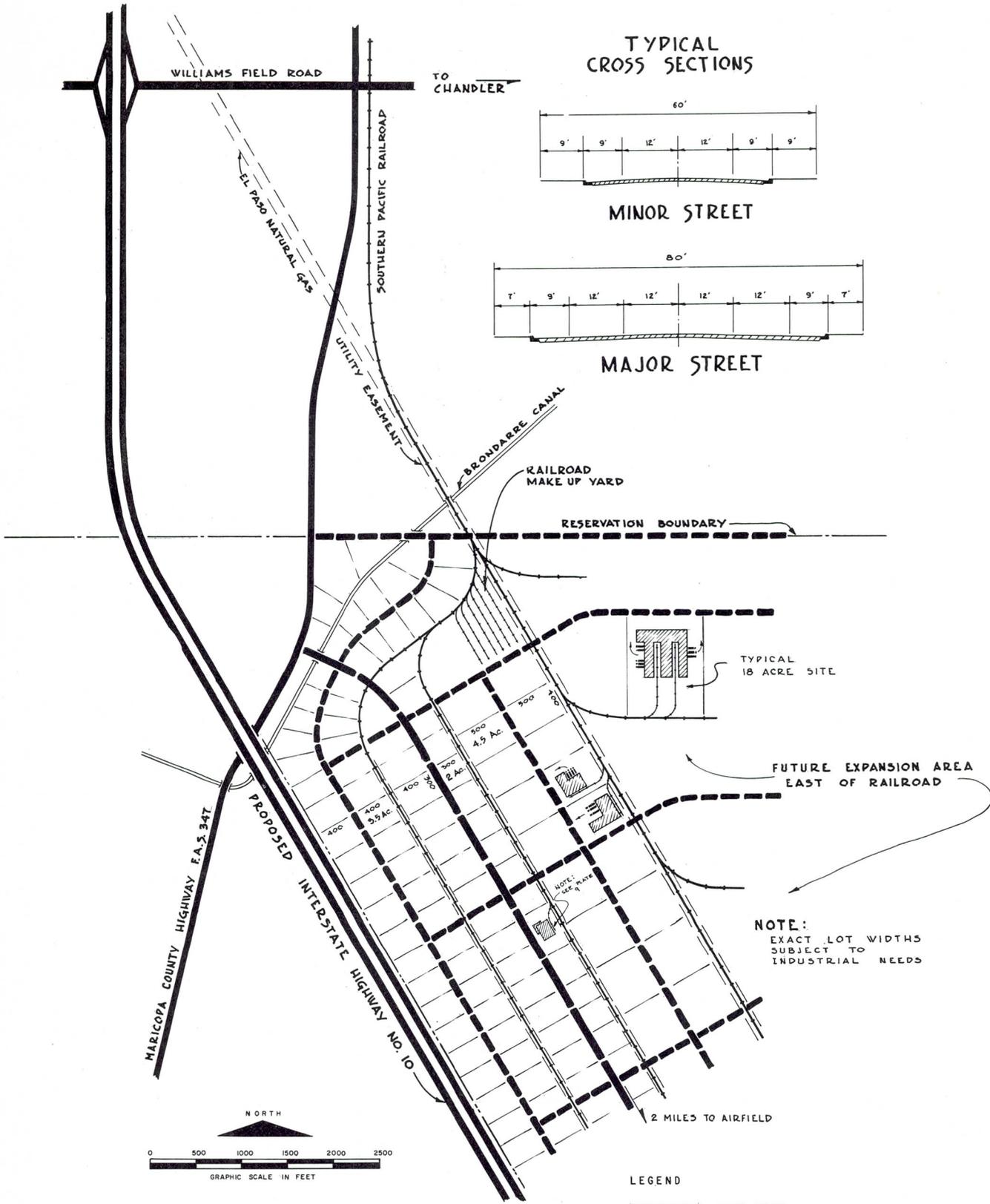


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JULY, 1961 H.E.R.

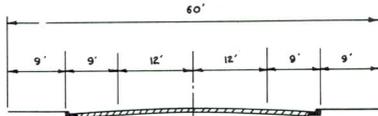
**LEGEND**

-  MAJOR STREET
-  MINOR STREET

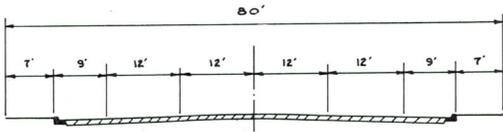
# SUGGESTED DEVELOPMENT PLAN AREA "C"



TYPICAL CROSS SECTIONS



MINOR STREET



MAJOR STREET

PREPARED BY THE ADVANCE PLANNING DIVISION  
 MARICOPA COUNTY PLANNING AND ZONING DEPARTMENT  
 JULY, 1961 H.E.R.

# SUGGESTED DEVELOPMENT PLAN

## AREA "A"

service. Plates 7 and 8 show suggested illustrative development plans for Area "A" and Area "C". Plate 9 represents a typical building-site plan which could be suitable for any of the industrial areas.

#### Agriculture

Approximately 3,800 acres of land have been reserved on the plan (see Sections 1, 2, 3, 10, 11 and 12) for some form of agricultural use. This represents an increase of more than 2,500 irrigated acres, since only Sections 1, 2 and 13 are presently under cultivation. Other vacant lands on the reservation should also be placed under cultivation if economically feasible.

#### Residential Development

Frequently, small "bedroom" type communities spring up around expressway interchanges near urban fringe areas. The major advantages, being the low time distant factor to major industrial areas and low land values. Should such a development occur in the Kyrene District, it should be encouraged to develop on lands situated upward and away from the major approach zones to the airfield. Desirable agricultural lands should not be used if possible.

An area suitable for residential development is shown on Plate 6. It contains approximately 1,500 acres of land capable of accommodating an urban population of about 10,000 persons. This should be carefully planned when warranted.

APPENDIX

APPENDIX A  
TYPICAL RESTRICTIVE COVENANTS

State of \_\_\_\_\_  
County of \_\_\_\_\_

KNOW ALL MEN BY THESE PRESENTS:

That \_\_\_\_\_ (Name of Industrial District Organization), a corporation acting herein by and through the undersigned officers duly authorized, is the owner of the hereinafter described property:

Being a tract of land out of the \_\_\_\_\_ Survey, Abstract \_\_\_\_\_, and being more particularly described as follows;

(Survey description omitted.)

does hereby, adopt the attached plat in four sheets as its plan of subdivision of the lands fully described therein and which shall be known as \_\_\_\_\_ Industrial District Number \_\_\_\_\_, an addition in the County, \_\_\_\_\_ County, \_\_\_\_\_ (State), and does hereby dedicate the streets and easementways as shown on said plat to the public and does also reserve the easements as shown thereon for utility installation and maintenance; and the undersigned does hereby restrict said property as hereinafter set forth, which restrictions shall be binding upon the purchaser or purchasers of any of said lots, lands or tracts and his or their heirs, assigns, successors, devisees

and administrators.

I.

All of the building sites in \_\_\_\_\_ Industrial District Number \_\_\_\_\_ are for industrial, commercial and warehousing purposes.

Building sites in this district may be used for such commercial, industrial and warehousing purposes, but grantee agrees not to use or allow the use of said premises or any portion thereof or any building or structure thereon at any time for the manufacture, storage, distribution or sale of any products or items which shall increase the fire hazard of adjoining property; or for any business which constitutes a nuisance or causes the emission of odors or gases injurious to products manufactured or stored upon adjoining premises or premises within five hundred (500) feet of said property; or for any purposes calculated to injure the reputation of said premises, or of the neighboring property; or for any purposes or use in violation of the laws of the United States or of the State of \_\_\_\_\_. Written approval by \_\_\_\_\_ (Name of Industrial District Organization), its successors or assigns, of a particular use shall be conclusive evidence of compliance with this or any other restriction.

II.

Buildings which may be erected on the above described property shall not be nearer than the building line as shown on plat along Freeway 183, or nearer than fifty (50) feet to any other established "frontage street" or any cross streets, or nearer than fifteen (15) feet to any side property line. All lots in \_\_\_\_\_ Industrial District Number \_\_\_\_\_ shall front on Freeway 183 or any other such "frontage streets" which may be projected and

established by \_\_\_\_\_ (Name of Industrial District Organization), to conform to a Master Plan for the entire \_\_\_\_\_ Industrial District at any time with or without notice to grantees and such streets will be the only "frontage streets" in this district, as that term applies in these restrictions.

### III.

Construction or alteration of all buildings in \_\_\_\_\_ Industrial District Number \_\_\_\_\_ shall meet the standards provided in these restrictions:

- (A) No building shall be constructed with wooden frame;
- (B) The front and side walls must be finished with face brick, their equivalent, or better, and also said face brick shall be to the finished grade on the front and to a depth of twenty (20) feet along sides;
- (C) All remaining walls shall be of common brick, or equivalent;
- (D) All other types of construction not covered in the above must first be submitted to and have the written approval of \_\_\_\_\_ (Name of Industrial District Organization), its successors or assigns;
- (E) Prior to construction or alteration, purchasers must submit two (2) sets of plans and specifications for such building to \_\_\_\_\_ (Name of Industrial District Organization), its successors or assigns, and written approval of such plans by \_\_\_\_\_ (Name of Industrial District Organization), shall be proof of compliance with the restrictions.

Plans and specifications for the construction, installation or alteration of all signs in setback areas, loading docks, parking facilities, and landscape planting in this district must first be submitted to and have the written approval of \_\_\_\_\_ (Name of Industrial District Organization), its

successors or assigns.

The erection of signs upon buildings shall be at the discretion of the building owner.

No accessory building use shall be construed to permit the keeping of articles, goods or materials in the open or exposed to public view. When necessary to store or keep such materials in the open, the lot or area shall be fenced with a screening fence at least six (6) feet in height; said storage shall be limited to the rear two-thirds of the property.

Employee, customer, owner or tenant parking will not be permitted on private or public dedicated streets in \_\_\_\_\_ Industrial District Number \_\_\_\_\_, and it will be the responsibility of property owners, their successors or assigns, to provide such parking facilities to the rear of any frontage street established setback areas. Parking areas must be paved with a year-round surface (asphalt or concrete).

Loading docks will not be permitted on \_\_\_\_\_ or any frontage street in District Number \_\_\_\_\_. Provisions for handling all freight, either by railroad or truck, must be on those sides of any buildings which do not face a frontage street.

Planting areas and landscaping shall conform to reasonable minimum standards as proposed by and on file with \_\_\_\_\_ (Name of Industrial District Organization).

The building codes of the City of \_\_\_\_\_ in effect at the time of any construction shall apply to such construction.

The owner of any tract in this district must at all times keep the premises, buildings, improvements and appurtenances in a safe, clean, wholesome condition and comply in all respects with all government, health

and police requirements; and any owner will remove at his own expense any rubbish of any character whatsoever which may accumulate on said property.

IV.

If, after the expiration of one year from the date of execution of a sale contract agreement on any lot within the district, any purchaser shall not have begun in good faith the construction of an acceptable building upon said tract, \_\_\_\_\_ (Name of Industrial District Organization), retains the option to refund the purchase price and enter into possession of said land. At any time, \_\_\_\_\_ (Name of Industrial District Organization), its successors or assigns, may extend in writing the time in which such building may be begun.

V.

No conveyance or deed unless it specifically describes it therein shall be construed to be a conveyance to any grantee of any right in \_\_\_\_\_ Road, \_\_\_\_\_ Road or \_\_\_\_\_ Road. \_\_\_\_\_ (Name of Industrial District Organization), retains and excepts the right to alter, change or close, partially or completely, the above named roads to conform to a master plan for the entire \_\_\_\_\_ Industrial District at any time with or without notice to grantees. \_\_\_\_\_ (Name of Industrial District Organization) retains the right to regulate and control the direction of traffic on streets in the district and the location or zoning of entrances or exits from properties into streets in the district until such controls legally become the responsibility of the City of \_\_\_\_\_, \_\_\_\_\_ County, \_\_\_\_\_ (State).

VI.

Each condition and covenant in the \_\_\_\_\_ (Name of Industrial District Organization) \_\_\_\_\_ Industrial District Number \_\_\_\_\_ shall terminate and be of no further effect on January 1, 2000 provided that at any time the owners of a majority of the acreage in \_\_\_\_\_ Industrial District Number \_\_\_\_\_ may, by written declaration signed and acknowledged by them and recorded in the Deed Records of \_\_\_\_\_ County, \_\_\_\_\_ (State), alter, amend or extend such restrictions, conditions and covenants and this right to so alter, amend or extend shall exist as long as the then owners of a majority of the acreage in said District Number \_\_\_\_\_ desire. The mere lapse of time shall not affect or alter the application of this section.

Invalidation of any of the foregoing restrictive covenants shall not affect the validity of any other of such covenants, but same shall remain in full force and effect.

EXECUTED this \_\_\_\_\_ day of \_\_\_\_\_, 19 \_\_\_\_.

\_\_\_\_\_  
(Name of Industrial District Organization)

By: \_\_\_\_\_  
President

ATTEST:

\_\_\_\_\_  
Secretary

(Seal)

The State of \_\_\_\_\_

County of \_\_\_\_\_

BEFORE ME, the undersigned authority, a Notary Public in and for \_\_\_\_\_ County, \_\_\_\_\_ (State), on this day personally appeared \_\_\_\_\_, President of \_\_\_\_\_ (Name of Industrial District Organization), known to me to be the person whose name is subscribed to the foregoing instrument, and acknowledged to me that he executed the same for the purposes and consideration therein expressed, in the capacity therein stated and as the act and deed of said corporation.

GIVEN UNDER MY HAND AND SEAL OF OFFICE, this the \_\_\_\_\_ day of \_\_\_\_\_, 19 \_\_\_\_.

\_\_\_\_\_  
Notary Public in and for

\_\_\_\_\_  
County \_\_\_\_\_ (State)

(Seal)

APPENDIX B  
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